

**City of Las Cruces, New Mexico  
Design Standards**

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## ARTICLE VI - TRAFFIC STANDARDS

### SECTION 6.1 Purpose

The purpose of this document is to regulate the various traffic related issues associated with public and private development such as access and median regulations, placement of traffic calming devices, installation of street lighting, placement of traffic control devices, and preparation of traffic impact studies.

### SECTION 6.2 Roadway Classification and Function

This section defines and characterizes City of Las Cruces street facilities, based on the intended function of the roadway. The function of a particular class of roadway is defined in terms of service to through traffic movements versus access to abutting properties. Access point design should be compatible with the function of the roadway since the number, spacing, type and location of access points has a significant effect on capacity, speed and safety. Construction of all access points require a building permit from the Community Development Department of the City of Las Cruces.

The approval of access permits will be granted, if both the design and the construction of driveways and median openings are compatible with the intended function of the roadway facility, as defined in this section. Designation of a particular street shall be determined by the following classifications:

1. Arterials: The primary function of an arterial is to provide high levels of mobility with limited or restricted access to abutting properties. Because mobility is the primary function of arterials, partial access control is highly desirable. However, when access is provided, regulations should limit the number, location, and design of access facilities.

2. Collectors: Collectors link neighborhoods and the other areas of homogenous land use with arterial streets. Collector streets, therefore, serve traffic movements between local and arterial streets, and serve through-traffic movement within local areas.

3. Local Streets: The primary function of local roads is to provide direct access to residences, homes and other abutting properties. The traffic generated by abutting properties are primarily short trips, or constitute a relatively short part of a longer trip connecting to roadways of higher classification.

### **SECTION 6.3 Driveway Classifications**

Driveway classifications on City streets are as follows:

1. Residential Driveway

A residential driveway is an entrance to and/or exit from a residential dwelling or dwelling adjacent to a dedicated City street, for the exclusive use and benefit of the owners.

2. Commercial Driveway

A commercial driveway is an entrance to and/or exit from any commercial/industrial/institutional business, or similar type of establishment providing access to employees and/or the public adjacent to a dedicated City street.

### **SECTION 6.4 Driveway Design Elements (Reference Exhibit I)**

Required design criteria are presented in this section to define access widths, spacing, and offsets.

a. Residential Driveways

1. Applicable in all residential areas which shall include all dwelling units, single family homes, and apartments.

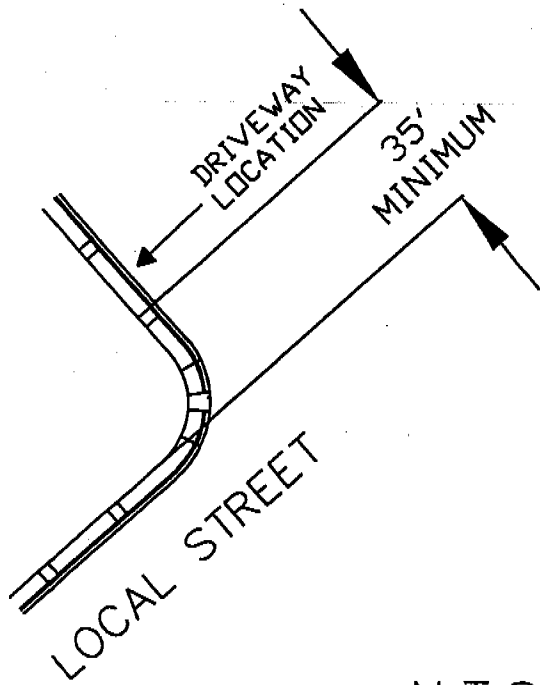
EXHIBIT I

City of Las Cruces

2003 TRAFFIC STANDARDS

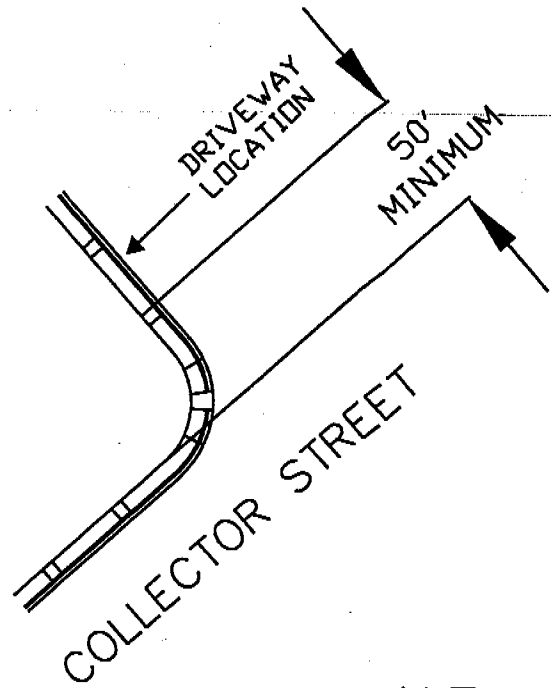
TYPICAL DETAIL: DRIVEWAY PLACEMENT

FROM LOCALS



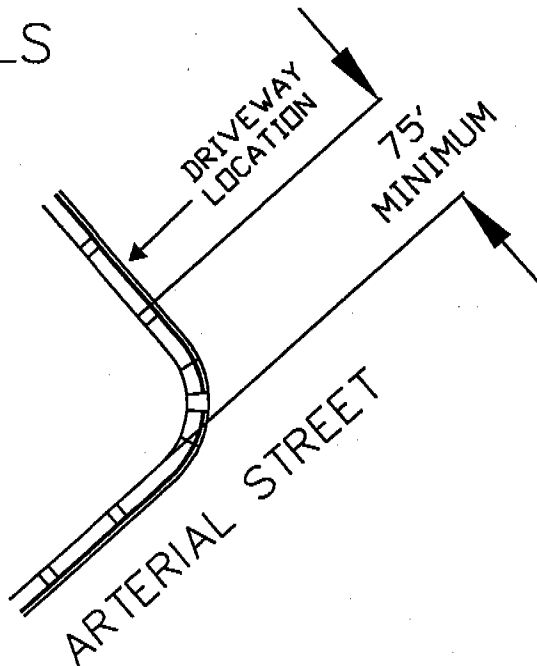
N.T.S.

FROM COLLECTORS



N.T.S.

FROM ARTERIALS



N.T.S.

2. Driveway widths shall be measured on the property line.
3. The minimum width of curb cuts shall be twelve feet (12') and the maximum width shall be twenty seven (27').
4. A minimum of twenty-two feet (22') of standing curb must remain between any two curb cuts located on the same lot or single parcel of land.
5. No double or circle driveways shall be installed or maintained, unless the property has a minimum of sixty feet (60') frontage on the affected residential street.
6. Corner property curb cuts shall be no closer than thirty five feet (35') from the property line at the intersecting local street, fifty feet (50') to the property line along collector streets, and seventy-five (75') to the property line along arterial streets. Under no condition shall driveways encroach on corner curb returns of intersections or within five feet (5') from handicap access ramps.
7. No driveway shall be closer than six feet (6') from side property lines, except when serving adjacent properties.

b. Commercial Driveways

1. Applicable to all commercially and industrially zoned properties which shall include commercial businesses, offices, and industrial areas.
2. Driveway widths shall be measured on the property line.
3. The width of two-way curb cuts shall be based on the classification of roadway which is being accessed.

<u>Street Accessed</u>	<u>Maximum</u>	<u>Minimum</u>
<u>Arterials</u>	<u>40 feet</u>	<u>30 feet</u>
<u>Collectors</u>	<u>40 feet</u>	<u>27 feet</u>
<u>Local Streets</u>	<u>35 feet</u>	<u>27 feet</u>

4. A minimum of fifty feet (50') of standing curb must remain between any

two two-way curb cuts located on the same lot or single parcel of land.

5. The width of one-way curb cuts shall be a minimum of fifteen feet (15') per driving lane.
6. A minimum of five feet (5') of standing curb must remain between one-way curb cuts.
7. Corner property cuts shall be no closer to the property line along the intersecting street than thirty-five feet (35') if the intersecting street is a local street, fifty feet (50') to the property line along collector streets, and seventy-five (75') to the property line along arterial streets.
8. No driveway shall be closer than five feet (5') from side property lines, except when serving adjacent properties.
9. The location of driveways serving adjacent properties should be reviewed to ensure that they do not create conflicting traffic movements or pose sight distance problems for turning traffic. Where feasible, driveways that serve adjacent properties should be combined into one driveway.
10. The access turning radii shall accommodate the turning radius of the largest design vehicle to routinely use the access.
11. A 50-foot minimum turning radius shall be used for an access when combination trucks or single unit vehicles exceeding 30 feet in length are intended to use the access on a daily basis.
12. Construction of all curb cuts is to comply with existing City of Las Cruces Design Standards as well as accessibility standards in place at time of building permit issuance.
13. The City shall determine if the proposed curb cuts or radius curb returns are adequate in accordance with existing or planned conditions and criteria.

c. Construction Costs For Driveways

The permittee is responsible for all costs associated with placement and/or

removal of driveways accessing public right of way. This is to include restoration to existing landscaping, road surfacing, and sidewalk area.

d. Exceptions

In unusual circumstances:

1. The City's Traffic Engineer may approve a driveway over twenty-five (25') in width in residential areas and thirty-five (35') in width in commercial areas. The request and approval must be in writing through the appropriate variance procedure. Disapprovals may be appealed to the Public Works Director;
2. The City's Traffic Engineer may approve curb cuts that do not meet the minimum curb cut distances from intersecting streets upon review of the site in question and/or a study showing projected traffic volumes, pedestrian volumes, and traffic impacts on existing facilities.

## SECTION 6.5 Median Openings

The location of openings in a median, to allow left-turn ingress and egress movements at a driveway or local street, will be based on the type and operating speed of the roadway, the volume of traffic expected to make the left-turn movements, and the location relative to other intersecting streets, driveways, and median openings. All median cuts or alterations shall be made at the time of request for a building permit, and shall be included on the construction plans.

a. Median Opening Priority

All city streets shall take priority over commercial access in determining location of median openings. Arterial streets shall take highest priority for city streets.

b. Traffic Analysis of Median Opening

To justify a new median opening, a traffic impact study which defines the ability of the roadway and the driveway to accommodate the opening shall be prepared by a Professional Engineer in the State of New Mexico, retained by the firm or individual requesting the median opening. The analysis shall include but not be limited to the following information.

1. The impact(s) to the operation at adjacent signalized and unsignalized intersections within 500 feet of the proposed opening shall be considered in the analysis.
2. Adequate sight distance requirements are being satisfied based on AASHTO criteria.
3. Analysis of potential conflicting vehicular movements with recommendations for eliminating those conflicts.

Shared access points for two or more adjacent individual parcels is highly encouraged. Consideration to future development of adjacent parcels must be considered in determining the opening location.

c. Minimum Median Opening Design Standards (Reference Exhibit II)

1. Minimum Spacing

A minimum median spacing of 300 feet from adjacent openings shall be required on all collector streets. On arterial streets, spacing shall be as follows.

<u>From Intersection with Arterial or Collector:</u>	<u>500 feet</u>
<u>All Other Openings</u>	<u>300 feet</u>

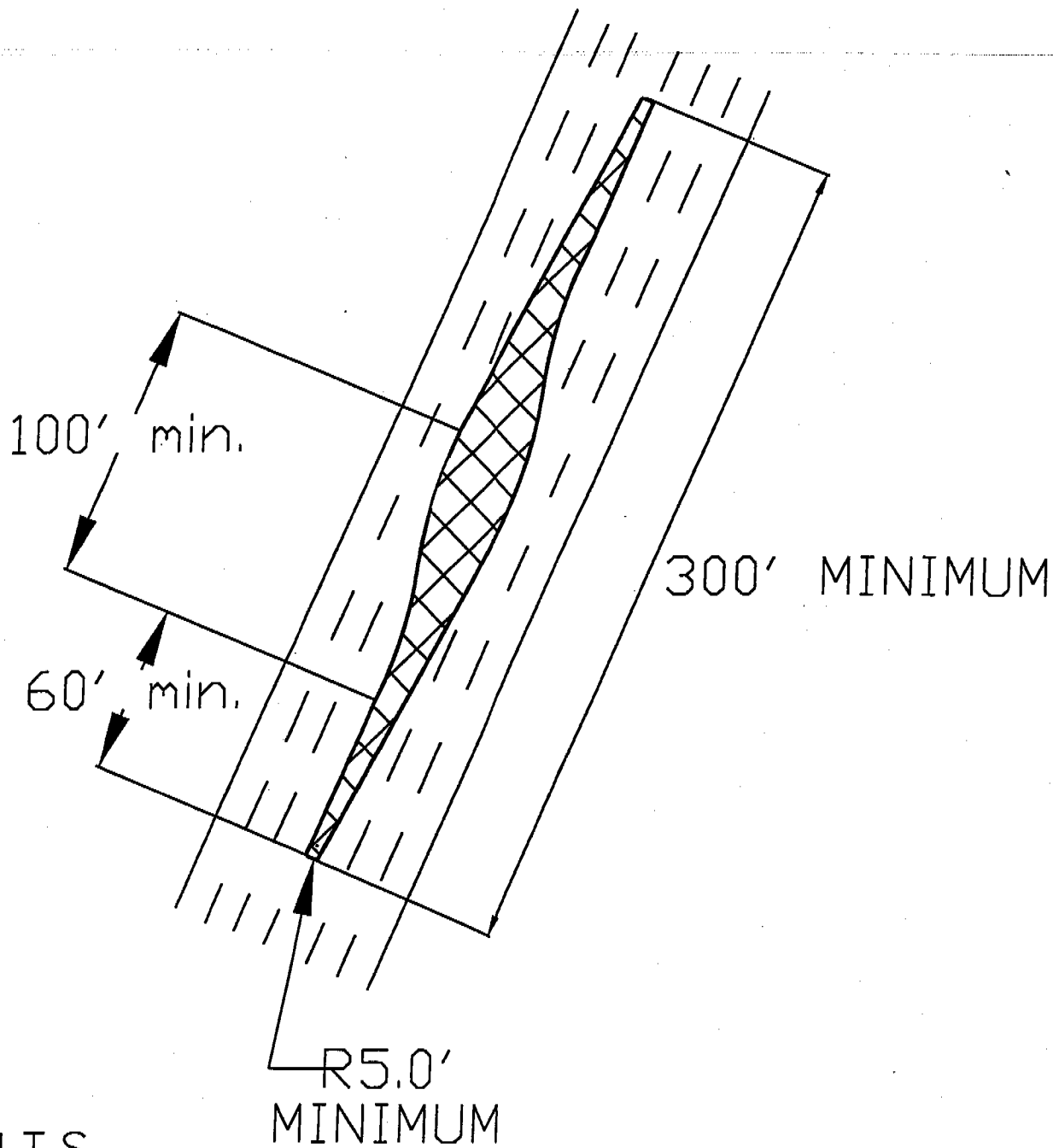
This distance shall be measured from face of curb to face to curb. Variance requests to the 500 foot requirement can be submitted to allow openings as close as 300 feet to arterial or collectors if justified.

EXHIBIT II

City of Las Cruces

2003 TRAFFIC STANDARDS

TYPICAL DETAIL: MEDIAN DESIGN



N.T.S.

2. Taper Length

A minimum deceleration taper of 100 feet shall be required on all median openings.

3. Storage Length

A minimum storage length of 60 feet shall be required on all median openings on City streets, unless the City Traffic Engineer requires additional storage length.

4. Commercial Access Requirements

In order for median openings to be considered for commercial access, the median opening must generate significant traffic, as demonstrated by a projected trip generation of not less than 25 left-turn ingress or 25 egress vehicles during the peak hour.

d. Median Opening Approval

New median openings and associated left-turn storage will be approved by the City Traffic Engineer upon receipt of a traffic impact study justifying the need for the median opening; the meeting of the minimum requirements in Section 6.9; and, a determination that the opening will not otherwise create a safety hazard.

Widening, relocating, or other alterations proposed to change or modify an existing median opening, for any reason, must be approved by the City Traffic Engineer, and must meet the requirements outlined in Section 6.6-6.9. All future median installations shall conform to this policy and shall be approved by the Traffic Engineer. Any variances requested to this policy must be submitted to the City Traffic Engineer for approval. Appeals to the Traffic Engineer's decision are to be made to the City's Public Works Director.

e. Construction Cost for Median Openings

1. In existing medians, the cost of constructing a new median opening for private access, for any reason, shall be paid by the party requesting the opening.

2. In existing medians, the cost of widening, relocating or otherwise changing or altering an existing opening, for any reason, shall be paid by the party requesting the alteration.
3. In existing medians, the cost of constructing a new median opening for access to dedicated existing public right-of-way, for any reason, shall be paid by the City.
4. In existing medians, the cost of constructing a new median opening for access to a new subdivision, for any reason, shall be paid by the developer.

f. Landscaping Consideration for Median Openings

In the event a median opening is approved by the City Traffic Engineer, and the existing median is landscaped, the Landscape Architect shall notify the following City staff in writing:

1. City Manager
2. Facilities Department\_Director
3. Public Works\_Director
4. Traffic Engineer

The party requesting the median opening shall be responsible for contacting the business or individual responsible for maintaining the landscaping on the median, if any, and securing written approval from the Public Works Director after providing a landscaping plan and a financial guarantee of performance, prior to beginning any construction on the median opening.

g. Appeals

In the event any applicant is aggrieved by the refusal of the Traffic\_Engineer to grant any permit for a driveway or a median opening or a variance to the clear sight triangle requirements of the Zoning Code, or in the event any owner or occupant shall be aggrieved by receipt of the notice of proposed revocation provided for in Section 5,

said applicant, owner, or occupant may, within thirty calendar days (30) after denial of said application or receipt of said notice, appeal the decision of the Traffic\_Engineer to the Public Works Director by filing written notice of appeal with the City Clerk within said time period.

h. Abandoned and Non-Conforming Driveways and Median Openings

This ordinance shall not be retroactive as to any driveways or median openings in existence and in actual use at the time of its adoption. After the adoption of this ordinance, if any vehicular, other sidewalk crossing, or median opening is clearly abandoned as a crossing, or is no longer used for its intended purpose, any permit previously or hereafter granted for said crossing or median opening as such shall be subject to revocation by the City Traffic\_Engineer, only after thirty calendar days (30) written notice of such proposed revocation has been given to the owner of the property to which the crossing or median opening is appurtenant. The owner's name and address will be determined from the Assessor's Office of Dona Ana County. Written notice also will be given to the occupant of said premises, if any, if such occupant is other than the owner. It shall be sufficient notice under the provisions of this and the following section to mail said notices by certified mail with return receipt requested. In the event the name and address of the owner cannot be reasonably ascertained from the Assessor's Office, and the premises are unoccupied, it shall be sufficient notice to publish said notice in a newspaper of general circulation in the City of Las Cruces, once a week for four (4) consecutive weeks. In the event the owner of the property is aggrieved by the revocation of any driveway or median opening, the owner shall appeal as outlined in Section 6.14.

i. Removal and Restoration Requirements

In the event the Traffic\_Engineer has revoked any permit for a driveway, use of a sidewalk crossing or median opening under the provisions of Section h. above, and previously restored, such crossing or median opening shall be removed by and at the expense of the owner of the property to which the driveway, crossing or median opening is appurtenant if the revocation is due to changes taking place within said property. The written notice of revocation shall also set forth requirements for the removal and proper restoration of the curb, sidewalk, or median opening. The time allowed for removal and restoration by the owner or occupant shall be thirty days (30) after notice in the cases provided for in Section h.; provided, however, that upon resolution duly passed by the City Council of the City of Las Cruces, that a traffic

